

20/08/2015
C173**SCHEDULE 10 TO THE DESIGN AND DEVELOPMENT OVERLAY**

Shown on the planning scheme map as **DDO10**.

DANDENONG ROAD (BETWEEN TOORONGA ROAD AND BATES STREET)**1.0 General design objectives**20/08/2015
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- To ensure development appropriately responds to the strategic context of this land.
- To ensure development makes efficient use of available commercial land.
- To ensure development does not unreasonably impact on the amenity of residential properties.

2.0 Buildings and works20/08/2015
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A permit is not required to construct a building or construct or carry out works for:

- Construction or carrying out the following within a commercial zone:
 - The installation of an automatic teller machine.
 - An alteration to an existing building façade provided:
 - The alteration does not include the installation of an external roller shutter.
 - At least 80 per cent of the building façade at ground level is maintained as an entry or window with clear glazing.
 - An awning that projects over a road if it is authorised by the relevant public land manager.
- Extensions to an existing dwelling within a residential zone.

New development must meet the design objectives and should address the design standards identified in this schedule.

Building heights (storeys) referred to in this Schedule exclude roof top gardens, architectural features and building services incorporated into the design of buildings. 'Building services' means functional plant and infrastructure including but not limited to air conditioning units, solar panels, roof mounted solar hot water units, masts, stairwells and lift over-runs.

Building heights are to be measured from the footpath at or near the mid-point of the relevant frontage.

In addition to other information required to be submitted with a planning application, if required by the responsible authority, applications must be accompanied by a statement detailing how the proposed development responds to the design objectives and design standards to be achieved under this schedule.

3.0 Precincts20/08/2015
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This clause includes specific objectives and standards for the precincts shown on *Map 1 - Dandenong Road Precinct Map* forming part of this schedule.

Precinct A

Preferred character

- Fronting Dandenong Road:
 - Higher-scale buildings that will contribute to a distinctive boulevard character.
 - A mix of uses at ground level that contribute to a vibrant public realm.
 - High quality, contemporary architecture that contributes to an attractive streetscape.
- On the corner of Dandenong Road and John and Boardman streets:
 - Mid-scale buildings that provide an appropriate transition in building heights, scale and setbacks to the development to occur on the southern side of John Street and the western side of Boardman Street.
- Fronting John and Boardman streets:
 - Lower-scale residential development that complements the existing character.

Design objectives

- To avoid underdevelopment of the land.
- To contribute to the safety and visual interest of the public realm.
- To complement and provide a transition to the lower-scale residential character of John and Boardman streets.

Design standards

Development should:

- Take vehicle access from Dandenong Road where possible.
- Locate and design car parking areas, substations and other services cabinets to minimise their visibility from the public realm.

Buildings should not visually overwhelm John Street and Boardman Street. This outcome may be achieved by using one or more of the following strategies:

- Locate the tallest buildings toward the Dandenong Road frontage.
- Provide slender and well-separated building forms.
- Step down in height towards adjoining residential properties and surrounding residential streets.
- Incorporate curved building footprints.
- Adopt a lightweight architectural expression.

Dandenong Road frontage and the corners of John and Boardman streets

Buildings should:

- Be built to the Dandenong Road property boundary.
- Be built to the John Street and Boardman Street property boundary, and provide an appropriate transition in height from development along Dandenong Road to the development fronting John and Boardman streets.
- Be orientated towards John and Boardman streets.

- Have a ground floor level generally at footpath level.
- Present active ground floor frontages to Dandenong Road with clear glazing for a minimum of 50% of the façade width.
- Be reduced in height and scale if they adjoin one or more properties at 6-12 John Street inclusive and that property remains developed as a single dwelling, to respect and protect residential amenity.
- Incorporate a continuous awning over the Dandenong Road footpath extending to approximately 750mm from the kerb and with its underside no higher than 3.6 metres above footpath level, and/or a colonnade within the property boundary.
- Incorporate well-articulated facades at lower levels (up to 10 metres high) and a visually interesting form at upper levels.

John Street and Boardman Street frontages (excluding corner sites)

- Buildings should not exceed a height of:
 - 10 metres (2 storeys) within 12 metres of the street frontage; and
 - 13 metres (3 storeys) between 12 metres and 20 metres of the street frontage.

Buildings should:

- Have a visually recessive third level when viewed from the other side of the street.
- Be set back from the street boundary:
 - 3 metres up to 7 metres in height.
 - 5 metres between 7 metres and 10 metres in height.
- Incorporate development of a residential character along the length of the street frontages, at all levels.
- Incorporate a modulated façade reflecting the rhythm of existing residential development in the street.
- Have a contemporary domestic architectural character.
- Have a ground floor level no more than 1 metre above footpath level.

Street frontages should incorporate:

- Front fences on the street boundary with a maximum height of 1.5 metres above the footpath level.
- Individual entries to ground level dwellings.
- Soft landscaping within the front setback of John and Boardman streets, including canopy trees along the Boardman Street frontage.

Precinct B

Preferred character

- Higher scale buildings fronting Dandenong Road transitioning to a low to medium scale at the rear residential interface.
- A mix of uses at ground level that will contribute to a vibrant public realm.
- High quality, contemporary architecture that will contribute to an attractive streetscape.

Design objectives

- To avoid underdevelopment of the land.
- To contribute to the amenity of the public realm of Dandenong Road.
- To reinforce the preferred character.
- To avoid unreasonable impacts on the amenity of residential properties to the north.
- To contribute towards traffic safety of rear lanes.

Design standards

- Buildings should not exceed a maximum height of 25 metres (7 storeys).
- Buildings should have a height at the Dandenong Road frontage of between 10 metres (3 storeys) and 20 metres (6 storeys).
- Above 20 metres (6 storeys) in height, buildings should be set back from the road frontage a minimum of 2 metres.
- Steps in building form should form an integrated part of an over-arching design concept.
- Northern and southern facades above a height of 10 metres (3 storeys) should be well articulated and adopt a lightweight architectural expression with a high proportion of openings and textured materials.
- Development should limit views into existing secluded private open space and habitable room windows of nearby residential properties.
- Development should not unreasonably prejudice the future development of adjoining properties.

Dandenong Road frontage

Buildings should:

- Have a ground floor level generally at footpath level.
- Present an active frontage to Dandenong Road with clear glazing for a minimum of 50% of the façade width.
- Incorporate a continuous awning above the footpath extending to approximately 750mm from the kerb and with its underside no higher than 3.6 metres above footpath level.

Properties abutting a laneway

Development should:

- Take vehicle access from the side street or rear lane where possible and avoid vehicle access from Dandenong and Waverley roads.
- Locate and design car parking areas, substations and other services at the rear to minimise their visibility from the public realm.

Buildings should:

- Incorporate screen landscaping within the northern setback at the ground and upper levels in order to soften the visual impact of the building; this could include the incorporation of vertical landscaping.
- Be set back from the laneway (north) property boundary at least:
 - 3 metres up to a height of 9 metres.

- Plus 0.3 metres for every metre of height over 9 metres up to 18 metres.
- Plus 0.5 metres for every metre of height over 18 metres (refer Figure 1).

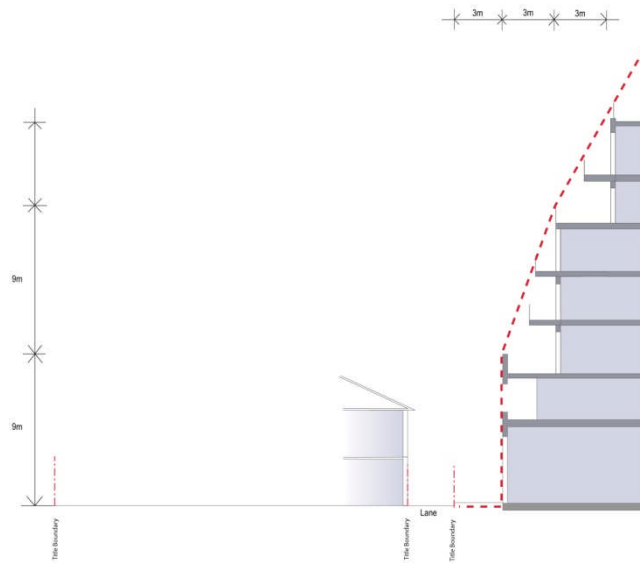


Figure 1: Setback diagram for properties abutting a laneway

Properties not abutting a laneway

- Where rear access is not available:
 - Vehicle access, substations and other services cabinets should be located on side streets where possible and be designed to minimise their visibility from the public realm.
- Buildings should:
 - Be set back from the northern property boundary at least:
 - 4.5 metres up to a height of 9 metres.
 - Plus 0.5 metres for every metre of height over 9 metres (refer Figure 2).
 - Incorporate screen landscaping within the northern setback at the ground and upper levels in order to soften the visual impact of the building; this could include the incorporation of vertical landscaping.

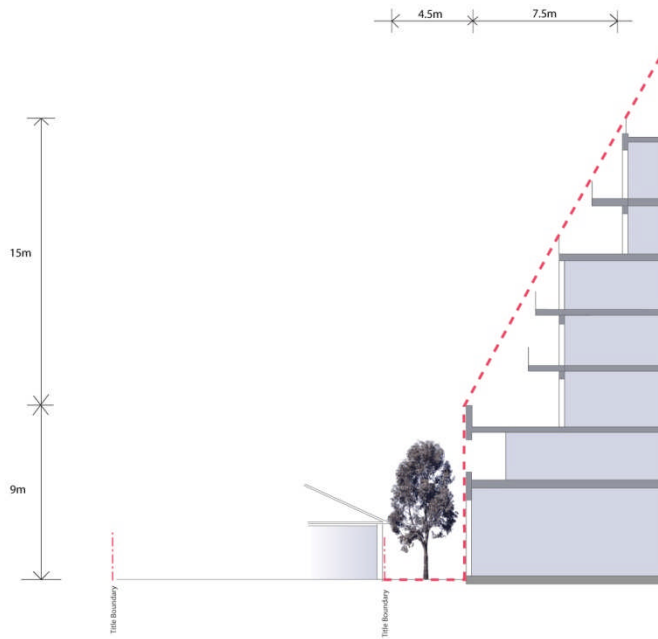


Figure 2: Setback diagram for properties not abutting a laneway.

4.0

Subdivision

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A permit is not required to subdivide land.

5.0

Decision guidelines

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Before deciding on an application, in addition to the decision guidelines in Clause 65, the responsible authority must consider whether the design objectives and standards of this schedule are satisfied.

6.0



Reference documents

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- *Dandenong Road Urban Design Framework (Between Tooronga Road and Bates Street), December 2012.*

Map 1 - Dandenong Road Precinct Map



	PRECINCT BOUNDARY		ZERO FRONT SETBACK
	COUNCIL BOUNDARY		3M FRONT SETBACK
	PRECINCT A		ZERO FRONT SETBACK AT CORNER SITES
	PRECINCT B		CORNER SITES
	STREET ADDRESS		LANEWAY
	RAIL LINE		
	CAULFIELD STATION		